



(Est. 1860)

DURHAM AMATEUR ROWING CLUB

City Boathouse, Green Lane, Old Elvet, Durham DH1 3JU

Durham Autumn Small Boats Head & Wear LDS Competitors Instructions

It is the competing clubs' responsibility to ensure all of its crews and Scullers abide by the British Rowing Row Safe, British Rowing Rules of Racing, Safeguarding and Protecting Children Policy and the specific local rules as presented in the Competitors Instructions. Specifically:

1. All equipment is safe and secure. Bow balls, heel restraints, boat buoyancy and all other safety measures as outlined in the British Rowing Row Safe must be in place. Random boat checks may be conducted throughout the event.
- 2.a All competitors must be suitably dressed for marshalling at the start and finish and may not be allowed to boat if the marshals judge they are inappropriately dressed.
- 2b Coxswains should be adequately dressed for the prevalent weather conditions and any possible changes. All coxswains must wear an approved PFD (Lifejacket or Buoyancy Aid) on top of other garments.
In 'front loader' boats only manually operated lifejackets can be worn (Row Safe 2.1)
3. Inexperienced crews must have an experienced coxswain.
4. Competitors are aware of the safety rules and competitors instructions and have been instructed in a capsizing drill.
5. Any competitor found by Umpires, marshals, launch crew or other officials without safety equipment will be returned to the nearest landing stage immediately and will be subject to possible disqualification as per the disciplinary procedures as outlined in the British Rowing Rules of Racing.
6. Competitors are responsible for proceeding safely, especially to the start and finish of their race. Competitors proceeding to the start or to their boat house must do so in single file, on the starboard side of the river.
7. Competitors are responsible for bringing to the attention of umpires, marshals, launch crew or other officials, any incident occurring during the event.
8. Competitors should stop and give assistance to any crew or competitor that is in danger in the water if a launch is not present.
9. Random checks of competitors racing licences will be carried out throughout the event and it is the competitors responsibility that s/he can provide evidence of his/her eligibility to race. Spot checks will be made on coxes weights.

10. **Marshalling and Boating.** Observe the marshalling plan (figure 2) and boat when called for by the landing stage official. Lowest numbers boat first.

Long Distance Sculls

Due to the large number of crews in the LDS boating will be split into two groups with Quads and doubles boating between 8.00am and 8.30am. The race for this group will start at 9.00am.

Singles will commence boating at 9am (if boating from DARC). Singles will be stacked in order starting opposite DARC and queuing upstream towards Maiden Castle. Once the doubles have cleared the course singles will proceed downstream in order to the start and assume marshalling as normal. Start for the singles will be 10.15am.

Autumn Small Boats Head

This division will run as one group and boating will be done starting with crew 1-10 at 11.15am. Race start time for the SBH will be 12.30.

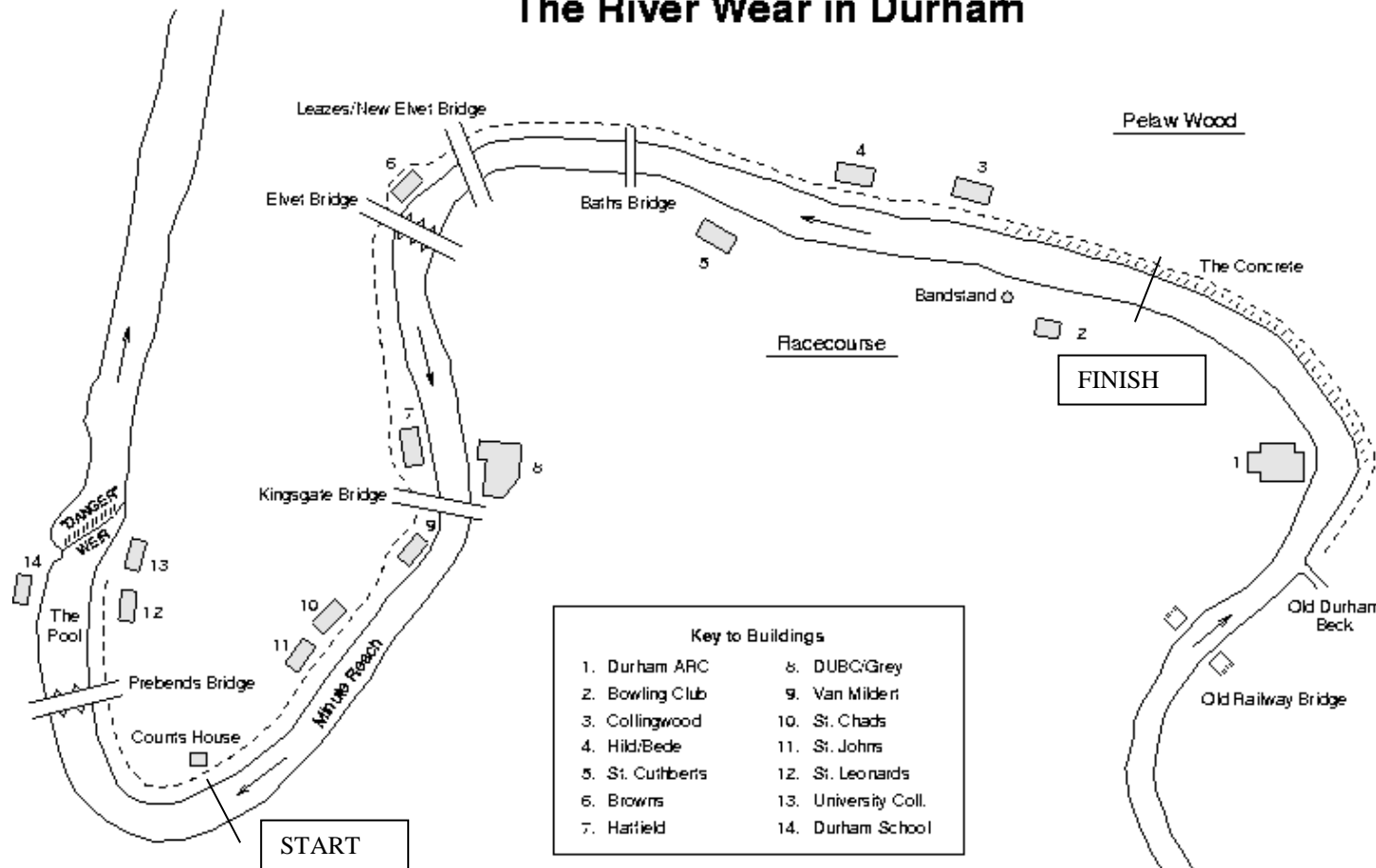
11. **Course and Obstacles.** Figure 1 shows a map of the course. Ensure you and your crew are familiar with the course. Figure 3 shows Elvet Bridge. Ensure your coxswain/steersperson is familiar with the instructions relating to the bridge.

Note:

The Prince Bishop river cruiser will commence sailing at 14.00 and whilst racing has been timetabled to finish before this time, crews returning to downstream boathouses after the SBH should not assume the river will be clear of other boats.

12. **Conduct of the Race.** Slower crews MUST give way by moving to the centre of the river. Time penalties may be awarded to competitors who fail to move, especially when instructed by officials. The use of foul language will be penalised.
13. In accordance with the British Rowing Rules of Racing, all crews must obey instructions of Umpires, marshals and safety launch crew. Launches have priority on the river at all times and competitors must note that they may have to travel to incidents with haste, but all effort will be made to prevent impeding racing crews. Racing may be stopped at any point due to an incident.
14. **SHALLOWS;** Upstream of finish, on inside of bend. Take care to avoid grounding. Obey marshalling instructions
15. **Welfare** – if competitors or officials have any concerns regarding welfare at the event these should, in the first instance, be raised with the Event Welfare Officer. Details of the Event Welfare Officer for this event can be found in the contact details at the end of this section.

The River Wear in Durham



Map prepared by Richard Mortimer (Durham ARC) - (c) August 1996
 based on a map by Jim Dulling (Durham ARC) 1994.

Figure 1: Map of the Wear

Umpires located at:

- Dunelm House landing stage
- Elvet Bridge (towpath downstream)
- New Elvet Bridge
- Baths Bridge

Marshals located at:

- UCBC/ St Leonard's Boathouse
- Durham School
- Before counts house
- Downstream of St John's BC Boathouse
- Hild Bede
- Above Finish
- Durham ARC landing stage

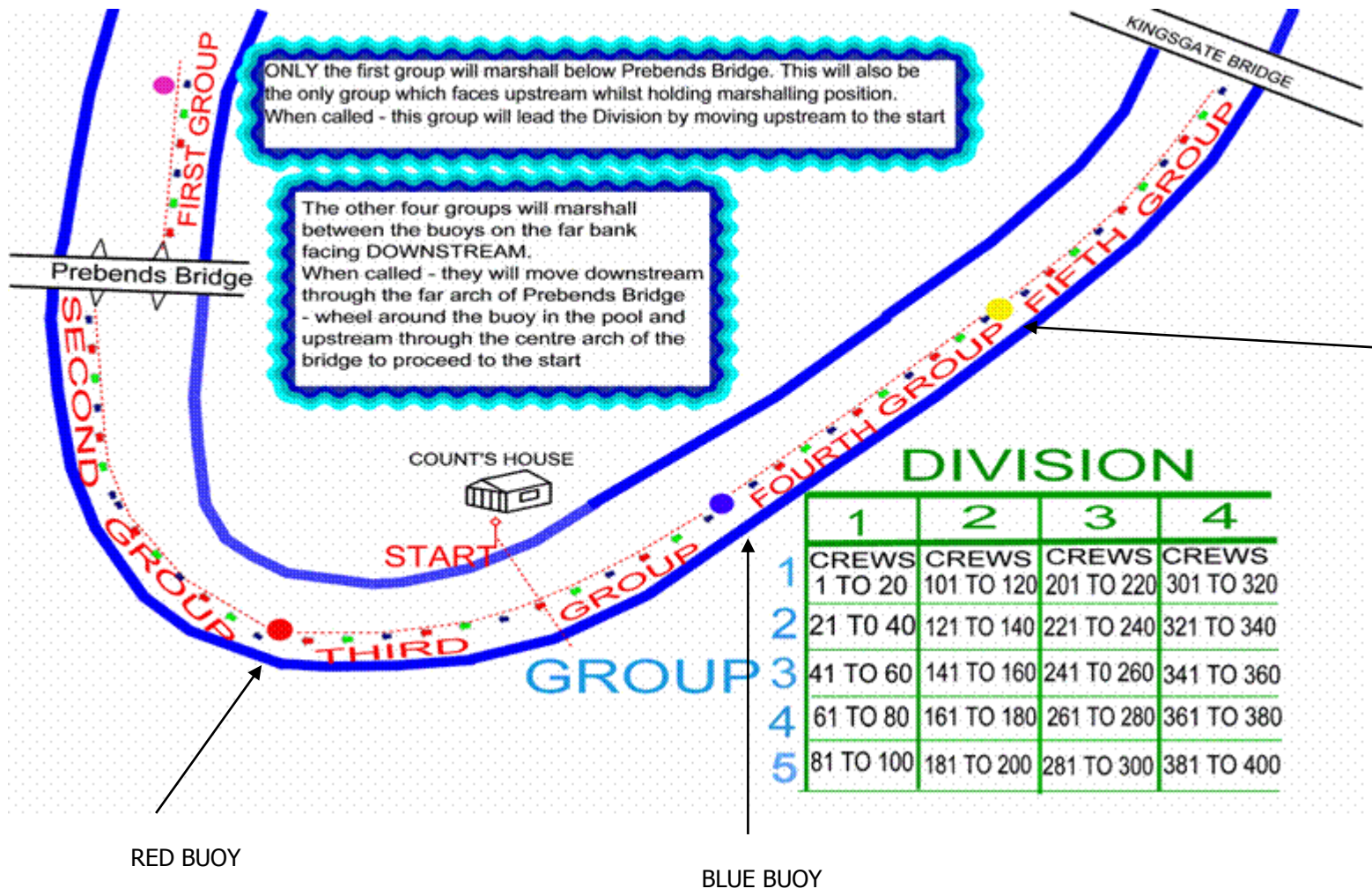
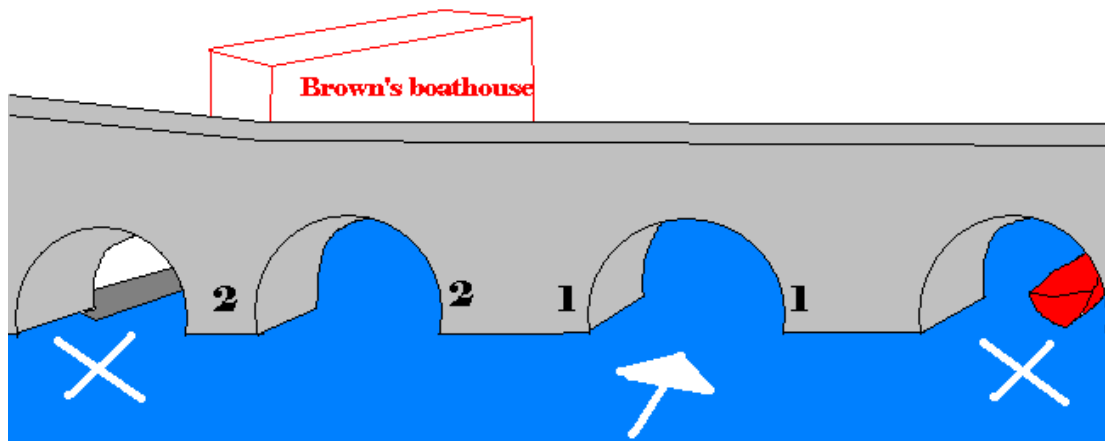


Figure 2: Marshalling Plan

Figure 3: Elvet Bridge



Elvet bridge is situated approximately half way up the course. There are only TWO arches that crews should use, numbered 1 and 2 as shown above (the diagram is viewed from the perspective of a cox travelling upstream in the race).

Arch number 1 is slightly wider and does not kink in the middle. It provides the best line and is easier to correct if you get it slightly wrong.

If you are instructed by officials, you must take arch number 2. This will require significant corrective steering in order to avoid the landing stage on the far side. This arch is narrower and kinks slightly in the middle, you will need to steer in the middle to get through.

Do not use either of the other arches. The one next to arch 1 often has a moored boat in it which you would hit. The one next to arch 2 does not provide enough space for boats as the landing stage actually goes into the arch. **Time penalties will be awarded against crews using these arches**

Please ensure all coxes, scullers and steerspersons are familiar with the arches and their numbers to ensure that they can follow umpires instructions when required.

3.1 Cycling

Durham ARC remind competitors, spectators and officials that cycling is not permitted between Brown's Boathouse ('The Boat Club') and Count's House and that public access along the riverbank has NOT been closed for this event. Cycling on the bank above Brown's Boathouse ('The Boat Club') can only be on the Pelaw Wood side cycle path. Durham ARC Regatta Committee accept no liability for any competitors, spectators or members of the public injured as a result of a cycling.

3.2 Club Contact

All competing clubs must provide Race Control with a contact number for use during the event in case of an incident involving their club.

3.3 Car Parking

No cars are to be parked in the Durham ARC car park. This area is for boat trailers only.

3.4 Contact Details and Officials

Race Control:
DARC Boathouse 0191 386 6431

Safety Advisor:
Paul Sowerby 07985899215

Welfare Officer
Vanessa Kelly 07837169511

Safety Boats:
Paul Sowerby 07985899215

Timing:
Richard Mortimer 07817223485

Co-ordinating Umpire:
Charly Curtis 07907 543 637

Durham Police 101

University Hospital of North Durham 0191 333 2333

Ambulance/Fire 999 OR 112

Prince Bishop Shopping Centre 0191 375 0416
(for access to Brown's Boathouse ('The Boat Club'))