



(Est. 1860)

## **DURHAM AMATEUR ROWING CLUB**

City Boathouse, Green Lane, Old Elvet, Durham DH1 3JU

### **Durham LDS & SBH 2016 Competitors Instructions**

It is the competing clubs' responsibility to ensure all of its crews and Scullers abide by the British Rowing Row Safe, British Rowing Rules of Racing and the specific local rules as presented in the Competitors Instructions. Specifically:

1. All equipment is safe and secure. Bow balls, heel restraints, boat buoyancy and all other safety measures as outlined in the British Rowing Row Safe must be in place. Random boat checks may be conducted throughout the event.
2. All competitors must be suitably dressed for marshalling at the start and finish and may not be allowed to boat if the marshals judge they are inappropriately dressed.
3. Coxswains should be adequately dressed for the prevalent weather conditions and any possible changes. All coxswains must wear an approved PFD (Lifejacket or Buoyancy Aid) on top of other garments.  
In 'front loader' boats only manually operated lifejackets can be worn (Row Safe 2.1)
4. Inexperienced crews must have an experienced coxswain.
5. Competitors are aware of the safety rules and competitors instructions and have been instructed in a capsizing drill.
6. Any competitor found by Umpires, marshals, launch crew or other officials without safety equipment will be returned to the nearest landing stage immediately and will be subject to possible disqualification as per the disciplinary procedures as outlined in the British Rowing Rules of Racing.
7. Competitors are responsible for proceeding safely, especially to the start and finish of their race. Competitors proceeding to the start or to their boat house must do so in single file, on the starboard side of the river.
8. Competitors are responsible for bringing to the attention of umpires, marshals, launch crew or other officials, any incident occurring during the event.
9. Competitors should stop and give assistance to any crew or competitor that is in danger in the water if a launch is not present.

10. Random checks of competitors racing licences will be carried out throughout the event and it is the competitors responsibility that s/he can provide evidence of his/her eligibility to race. Spot checks will be made on coxes weights.
11. Marshalling and Boating. Observe the marshalling plan (figure 2) and boat when called for by the landing stage official. Lowest numbers boat first.

Durham LDS – Start time is scheduled for 9.30 with boating starting at 8.20am with no 1-10.

Durham SBH Head – Start time scheduled for 12.30pm with boating from 11.20am with no 210 - 210

12. Course and Obstacles. Figure 1 shows a map of the course. Ensure you and your crew are familiar with the course. Figure 3 shows Elvet Bridge. Ensure your coxswain/steersperson is familiar with the instructions relating to the bridge.

The Prince Bishop will be moored on the outside of the bend once crews have navigated Elvet Bridge. It will not sail during the event but does have a scheduled sailing planned at 2pm.

13. Conduct of the Race. Slower crews MUST give way by moving to the centre of the river. Time penalties may be awarded to competitors who fail to move, especially when instructed by officials. The use of foul language will be penalised.
14. In accordance with the British Rowing Rules of Racing, all crews must obey instructions of Umpires, marshals and safety launch crew. Launches have priority on the river at all times and competitors must note that they may have to travel to incidents with haste, but all effort will be made to prevent impeding racing crews. Racing may be stopped at any point due to an incident.
15. **SHALLOWS; Upstream of finish, on inside of bend. Take care to avoid grounding. Obey marshalling instructions.**
16. Welfare – if competitors or officials have any concerns regarding welfare at the event these should, in the first instance, be raised with the Event Welfare Officer. Details of the Event Welfare Officer for this event can be found in the contact details at the end of this section.

Figure 1: River Wear Plan

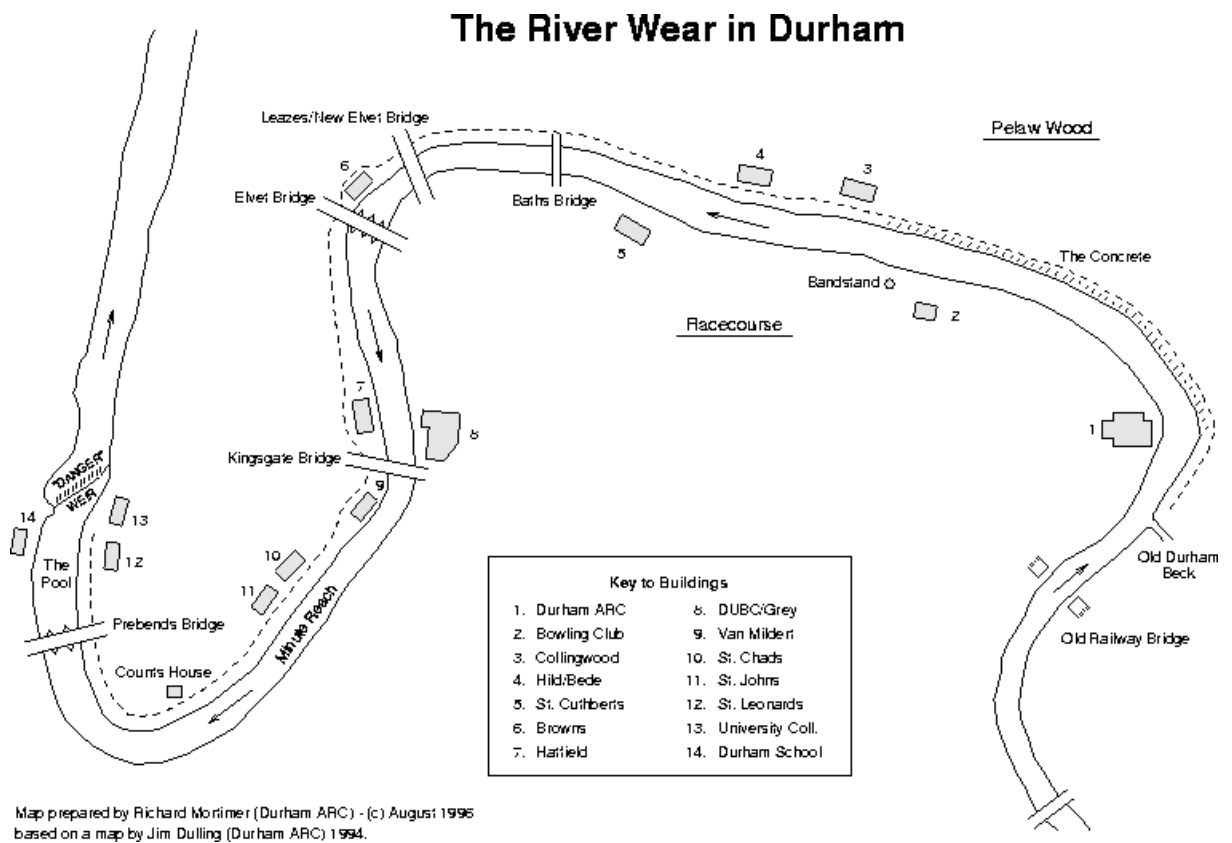


Figure 1: Map of the Wear

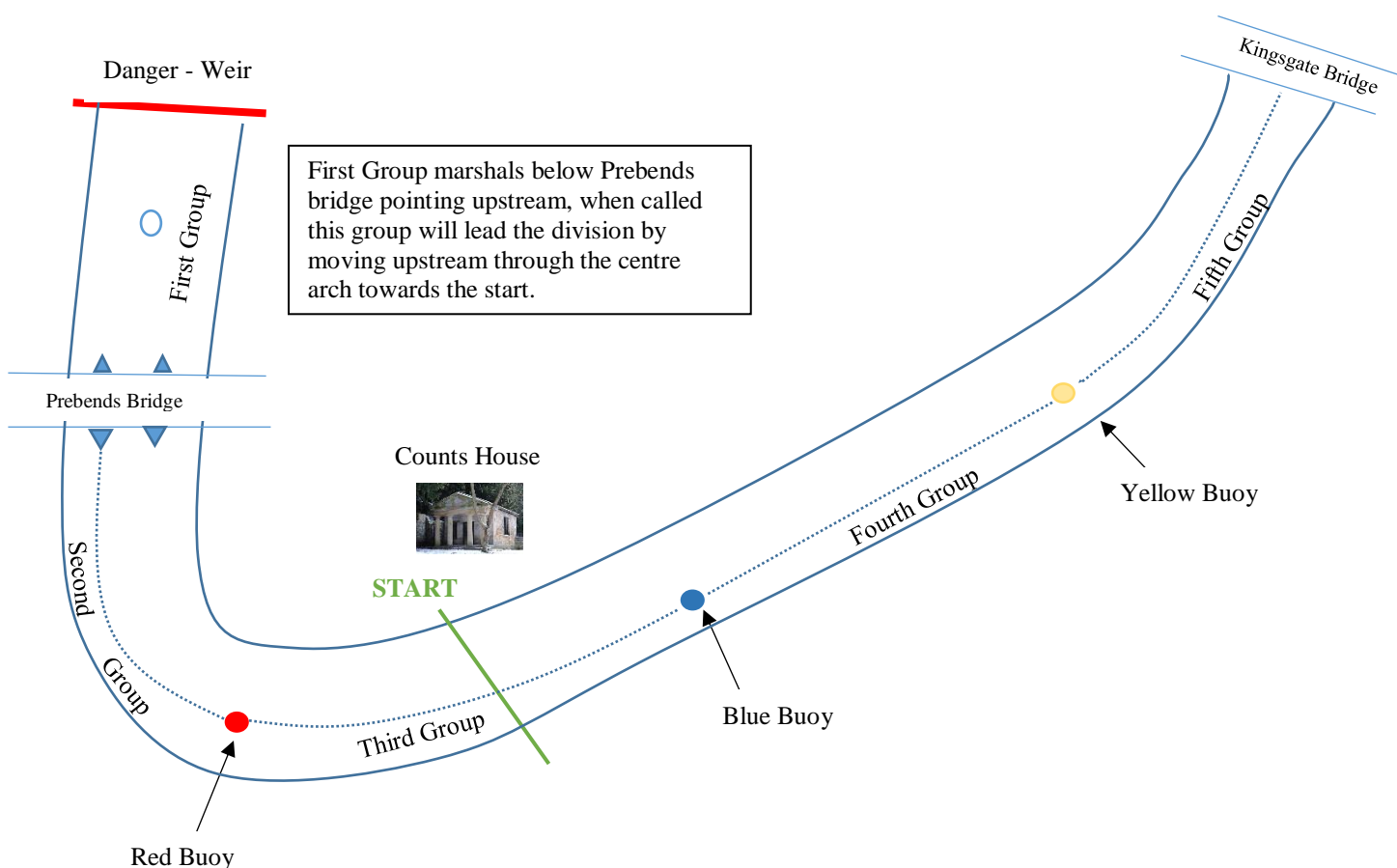
Umpires located at:

- Dunelm House landing stage
- Elvet Bridge (towpath downstream)
- New Elvet Bridge
- Baths Bridge

Marshals located at:

- UCBC/ St Leonard's Boathouse
- Durham School
- Before counts house
- Downstream of St John's BC Boathouse
- Above Finish
- Durham ARC landing stage

Figure 2: Marshalling Plan

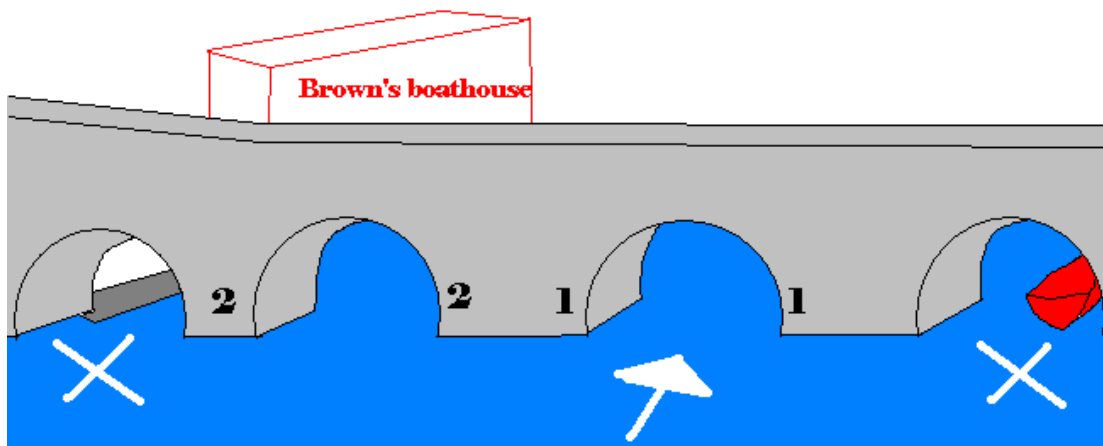


First Group marshals below Prebends bridge pointing upstream, when called this group will lead the division by moving upstream through the centre arch towards the start.

Groups 2 – 5 will marshal between the boys facing downstream (unless instructed otherwise). When called they will move downstream through the left hand arch of Prebends bridge, turning around the white buoy and upstream through the centre arch to proceed to the start.

		Division			
		1	2	3	4
Group	1	Crews 1 to 20	Crews 201 to 220		
	2	Crews 21 to 40	Crews 221 to 240		
	3	Crews 41 to 65	Crews 241 to 265		
	4	Crews 65 to 95	Crews 265 to 290		
	5	Crews 95 to 140	Crews 290 to 325		

Figure 3: Elvet Bridge



Elvet bridge is situated approximately half way up the course. There are only TWO arches that crews should use, numbered 1 and 2 as shown above (the diagram is viewed from the perspective of a cox travelling upstream in the race).

Arch number 1 is slightly wider and does not kink in the middle. It provides the best line and is easier to correct if you get it slightly wrong.

If you are instructed by officials, you must take arch number 2. This will require significant corrective steering in order to avoid the landing stage on the far side. This arch is narrower and kinks slightly in the middle, you will need to steer in the middle to get through.

**Do not** use either of the other arches. The one next to arch 1 often has a moored boat in it which you would hit. The one next to arch 2 does not provide enough space for boats as the landing stage actually goes into the arch. **Time penalties will be awarded against crews using these arches**

### 3.1 Cycling

Durham ARC remind competitors, spectators and officials that cycling is not permitted between Brown's Boathouse ('The Boat Club') and Count's House and that public access along the riverbank has NOT been closed for this event. Cycling on the bank above Brown's Boathouse can only be on the Pelaw Wood side cycle path. Durham ARC Regatta Committee accept no liability for any competitors, spectators or members of the public injured as a result of a cycling.

### 3.2 Club Contact

All competing clubs must provide Race Control with a contact number for use during the event in case of an incident involving their club.

### **3.3 Car Parking**

No cars are to be parked in the Durham ARC car park. This area is for boat trailers only.

### **3.4 Contact Details and Officials**

Race Control:  
DARC Boathouse                      0191 3866431

Safety Advisor:  
Richard Castling                      07870973967

Safety Boats:  
Peter Bradley                          07710451601

Welfare Officer:  
Stephanie Snowball                  07761089183

Timing:  
Richard Mortimer                      07817223485

Co-ordinating Umpire:  
Charly Curtis                          07907543637

Durham Police                          101

University Hospital of  
North Durham                          0191 3332333

Ambulance/Fire                        999 or 112

Prince Bishops Shopping  
Centre                                      0191 3750416  
(access to Brown's Boathouse)